

## News from the members

### ● Helsinki YTV evolves into HSL

Helsinki Regional Transport Authority HSL starts operation on 1 January 2010.

The new transport authority merges YTV Transport and the planning and procurement functions of HKL (Helsinki City Transport), it is responsible for the preparation of the Helsinki Region Transport System Plan. Presently the members municipalities of the joint authority are Helsinki, Espoo, Vantaa, Kauniainen, Kerava and Kikkonummi. More municipalities will join later.

For more: [www.hsl.fi](http://www.hsl.fi)

### ● Valencia eTM changes into Agencia Valenciana de Movilidad Metropolitana

As of 1 January 2010, the Metropolitan Transport Authority of Valencia (eTM) just nine years old, enlarges its scope over other metropolitan areas of the valencian region such as Alicante and Castellon including taxis services. The new integrated agency Agencia Valenciana de Movilidad Metropolitana has been entrusted by the Regional Government of Valencia Region and the composition of the Board will reflect the new scope and competencies. Agencia Valenciana is seen as the best answer to the challenging changes in the public transport system of the region.

For more: [www.etmvalencia.es](http://www.etmvalencia.es)

### ● The transport authority of the metropolitan area of Lisbon LTMA,

has been settled. It entered service since July 2009. The main first missions of the Authority are the Bill of Transport for the metropolitan area of Lisbon, the elaboration of the urban mobility plan and the mobility survey. In the next years, the major topic will be the implementation of the regulation 1370/2007 on passenger services by rail and by road (also called the Public Service Obligation regulation).

LTMA has announced it will join full membership at EMTA in 2010.

For more: [cgaivoto@imtt.pt](mailto:cgaivoto@imtt.pt)

To receive  
this newsletter  
by e-mail:  
[contact@emta.com](mailto:contact@emta.com)

## News from the cities

STIF: Innovative rolling stock in Ile de France to improve travel experience

### ● Francilien the new electric multiple unit train entered service in Ile de France on 13 December 2009.

Francilien is part of the rolling stock renovation policy of STIF the transport authority: 172 trainsets will be delivered within April 2015, they will allow the discard of 40 years old push-pull trainsets no longer able to meet reliability standards and passenger comfort expectations.

Francilien features innovation and leading-edge technology. Car bodies are just 13.24m long but wider than usual (3.06 m), the extra 0.24m allowing larger seats and larger centre aisles which helps passengers getting around and finding a place. Open inter-car gangways give a view along the length of the train reinforcing the feeling of security. The LED lighting is a striking feature of this train, it offers the possibility of changing lighting effects and is much sober in energy consumption. Underfloor heating and air conditioning ensure comfortable temperature.



Passenger information will be displayed on flat screens showing a dynamic route map with the next stop and the final destination and simultaneous audio information will be provided.

Accessibility for all is achieved at end cars with a retractable flap to permit level access from 920mm height platforms. Where station platforms are lower, folding steps are provided. As the majority of station platforms are lower (550mm), a costly programme of works, within the framework of the Accessibility Master Plan, is launched to raise the platforms to 920mm above rail. It starts with line Paris-Luzarches, the first line where Francilien enters service.

Continuous studies are undertaken to extend the Francilien service on the east part of the rail network (Paris-Est) in July 2012 and on the west part (Paris Saint Lazare) in January 2013. Besides raising platforms, works in stations consist of implementing beacons (to inform the train about the platform height inducing for example the unfold of the folding steps to accommodate lower platforms) and other necessary signing.

The introduction of the 172 trainsets is also an opportunity to reallocate according to the Rolling Stock Master Plan, trainsets still worth ten to twenty years more duty. In 2016, the whole fleet will be either new or recently refurbished.

The SNCF double-decker cars will be gradually reallocated on the longer routes where passenger expect to travel seated as opposed to standing, while Francilien will be mostly dedicated to close suburbs routes where higher frequency provides for capacity.

Francilien design integrates the logos of SNCF (bright red) and STIF (blue and orange on silver) on a white coat background.

The purchase from Bombardier of 172 trainsets as firm order amounts to €2 bn, 50% paid by STIF and 50% by SNCF. Studies are pursued to evaluate further needs.

#### Francilien key figures

- > 172 trainsets (1321 vehicles),
- > length of 95m (seven-car train) or 112.5m (eight-car train),
- > body width 3.06m,
- > maximum speed 140km/h,
- > overall capacity with a standard of 4 people/square-meter is from 760 passengers (seven-car train) of which 304 seated, to 920 passengers (eight-car train) of which 380 seated,
- > Delivery from 2009 to 2015 (40 trainset/year).

For more: [www.stif.info](http://www.stif.info)

● **T ZEN concept project... more than a bus**  
 Bus remains the favourite mode of transport in dense urban areas, according to EMTA Barometer<sup>1</sup>. These past ten years however, have seen tramways back into cities. They gained increasing popularity thanks to their elegant design, silent power energy and smooth sliding into the densest areas of our busiest cities at times on a green grass carpet.

Tramways though need important and costly infrastructure work, they disrupt the local traffic for months if not years during construction, and

call for a significant number of passengers which can be met only on specific routes and not everywhere even in a dense urban area.



The innovative concept-bus T ZEN project in Ile de France, under the initiative of STIF, challenges the Tramway services and yet adapts itself to smaller conurbations and lower patronage.

T ZEN falls under the High Quality Bus Service, it retains the efficiency of space allocation resource, and aims to particularly serve average dense areas-where urban planning is developing-and places to and from which mobility flows are important. T ZEN is a system-service. It operates exclusively on dedicated lanes and serves stations that respond to a highly effective and modern design, clearly recognisable by the travellers. The economic efficiency of T ZEN is supported by the optimization of quality criteria related to punctuality, high frequency and reliability, travel information display and user friendly design.

#### T ZEN key features

- > dedicated bus lanes, possibly transformed later on into tram tracks if capacity requires,
- > stations designed alike tramway stations and in average 500m apart,
- > advanced ITS for right of way at intersection and real time information on board and at stations,
- > design-for-all features ensure level access to stations,
- > higher Euro V environment standards compatible,
- > high quality of service criteria,
- > 10 pilot lines will be deployed in a first phase,
- > construction of a first lane starts in 2009, service to be opened in Spring 2011.



More than a bus, the innovative T ZEN-concept mode will provide reliable mobility speed and enjoyable travel experience. It will bring an economic-efficient response to the peripheral and/or new urban schemes, reinforcing the structure and optimizing the connections of the existing bus network while offering all the adaptability of bus services.

<sup>1</sup> EMTA Barometer is a benchmark of transport networks performances across EMTA members for more [www.emta.com](http://www.emta.com) section Publication

For more: [www.stif.info](http://www.stif.info)

## News from European projects

### ● BEST Benchmarking European Service of Public Transport.

BEST is a ten years old initiative that aims at increasing the use of public transport through the updating of information on how public transport systems perform in the eye of the customers.

Since 1999, the BEST team project has built a network of professionals and developed unique expertise. A new web base reporting solution has been introduced and several thematic workshops are held each year. BEST chairman is Per Gellert from MOVIA Copenhagen and several members of BEST are cities of the EMTA network.

The new campaign for recruiting BEST partners<sup>1</sup> or BEST members<sup>2</sup> will end 29th January 2010.

For more information on the 2009 BEST report, on main events, on costs and registration form, please visit <http://best2005.net/>

<sup>1</sup> Partners: Fees are €3000 and include access to web report, password protected part of the BEST web site, annual BEST Seminar and workshops.

<sup>2</sup> Members: Fees €19,000 and up and include data collection, analysis & reports, access to web report, the password protected part of the BEST web site, annual BEST Seminar and workshops.

### ● AENEAS

Attaining ENergy Efficient mobility in an Ageing Society is a European project that aims at improving the attractiveness of sustainable transport and contribute to modal shifts towards energy-efficient modes of transport among older people.

Aeneas partners think that those who should benefit most from a reliable public transport system very often are unable to take full advantage because it is too complicated, not really accessible or too expensive. Therefore AENEAS places the emphasis on the training programmes for the elderly in partner cities.

AENEAS held a second workshop in 2009 in San Sebastian (Spain). The topic was "Walking and traffic safety in an ageing society". Target groups of AENEAS training workshops are professionals from city mobility departments, public transport operators, representatives from NGOs working on the theme of sustainable transport.

Several key note speeches have been delivered notably on the thematic of the impact of ageing on walking abilities and the related safety issues outdoors from the INGEMA gerontology institute and the thematic of Inclusive design for outdoors environment (I<sup>2</sup>DGO). It has been acknowledged by researchers that a "supportive environment" with large and flat walk path, trees and benches every now and then are strong incentives for older people to go out and about.

The safety of older people's mobility can be improved by encouraging them to use more

public transport, and economic incentive such as reduced senior fares are important. But reaching the next transport stop is a walking activity and it cannot be neglected that the accessibility, the maintenance and the quality of urban infrastructure have an impact on mobility behaviour. We have to remember that older people are the most vulnerable with regard to pedestrians fatalities.

San Sebastian workshop was the opportunity to observe good lay-out of pedestrian facilities, large walking path, numerous benches to rest and good traffic crossing organisation. Some suggested improvements have been discussed among the group.

The next workshop will take place in Munich on 4-5 March 2010 on the topic of "Multimodal marketing activity in an ageing society".

*Last minute info: AENEAS project Partner ZGB and local public transport provider Stadbus are the winners of the Mobility Award in Salzburg (Austria).*

For more: [www.aeneas-project.eu](http://www.aeneas-project.eu)

### ● EMTA members team up in the European project CAPRICE

CAPRICE aims at exchanging experience between public transport authorities (PTAs), local and/or regional authorities, and transport operators. CAPRICE will identify and promote good practices, help transfer of knowledge and implement a pilot demonstration where the setting up of a new public transport authority or similar institution integrating public transport services is planned.



CAPRICE « *CAPital Regions Integrating Collective transport for increased energy Efficiency* » is a European project within the framework of the 2007-2013 INTERREG IV C Programme of European territorial co-operation. It was launched on 18 February 2009 in Berlin's Town Hall when the five partners ZTM Warsaw (Poland), SP Vilnius (Lithuania), City of Bucharest (Romania), STIF Paris-Ile de France (France) and VBB Berlin-Brandenburg (Germany) decided over the objectives and different steps of the project. The total cost of the project amounts to 1,110,367.95 € with an expected funding awarded by the European Regional Development Fund of 885,467.95 €.

Partners regularly meet in workshops, each of them hosting in turn. Three workshops already happened in 2009: on institutional issues, on contractual framework and on energy-efficient vehicles. Four workshops are planned for 2010, they should deal with the following topics: travel information, efficient procurement, fares and subsidies, and ticketing services.



With the exception of Bucharest, all CAPRICE partners are EMTA members. All along the 2009 meetings and workshops, they reinforced mutual links and came to share common vision on responsibilities and ways to handle the urban transport authority's missions. Here is a short report on the work of the partners.

### ● About organisation and financing of public transport Vilnius 3-5 June 2009

The workshop was the opportunity to exchange on the institutional framework for organisation, management, infrastructure development and financing of public transport services. Partners reached common thoughts on some main points.

- > Partners think it is suitable that local and regional authorities endorse political and administrative responsibilities for integrated public transport services.

Clear statements and explicit responsibilities at all levels, political, administrative, operational, as well as efficient decision-making processes, are crucial elements for an effective organisational framework.

*Being yet part of EMTA network resulted in stronger and more dynamic working relations.*

*Jean Christophe Monnet STIF Ile de France*

- > All modes (regional rail, underground, tram, buses and ferries) of public transport in a given region should be managed by one integrated authority that should define the framework in which one or more operators must operate the services. Key tasks like overall operational planning, tariff setting, quality standard monitoring, sales and marketing, customer information, infrastructure planning have to be integrated. The authority should also control the operators' performance with regard to the services required and the economical efficiency and monitor passenger expectations.
- > Partners stress that sufficient and reliable compensation are needed. Public transport costs usually cannot be covered only by fare revenues. Therefore a stable and reliable public funding is desirable, and social fares where they exist, must be compensated to operators. Such specific mechanism like the "transport tax" (specific tax on employers) in France, is seen as an efficient and significant contribution to the coverage of the public transport costs.
- > Partners also acknowledge the help Regulation 1370/2007<sup>1</sup>, that came into force December 2009, provides as a harmonised legal framework.

<sup>1</sup> Regulation 1370/2007 « of the European Parliament and of the Council on passenger transport services by rail and by road ».

*The success comes from the fact that (almost) all participants are EMTA members, we know each other quite well, problems we face are similar to all of us, so it enables us to go deeper and analyse very specific topics"*

*Viktorija Puzaite SP Vilnius*

#### About tendering and contracting in passenger transport services, Warsaw 14-16 September 2009

Regulation 1370/2007 was precisely the topic of this workshop. The following observations have been shared.

- > Competitive tendering and contracting have turned out to be an efficient instrument for raising the quality of public transport services and the related passenger satisfaction while reducing the amount of public subsidies needed.
- > A strong and dedicated entity body that prepares the tenders, defines the level of service the passenger aims at, carries out the tendering procedure and controls the contracts, is desirable. This is especially the case in capital-regions where there is a long tradition of strong public operators (historical operators).
- > Cities also have the possibility to directly award contracts to their own public operator. Even in the case of short term contracts directly awarded, it is suitable that clear and explicit definition of the authorities' tasks and the operators' ones are laid out and that complex quality control systems (bonus/malus



mechanism for example) is provided, in order to put the pressure on the operators to deliver a high quality service.

*"... it was a tremendous opportunity to discuss the recent challenge we all faced with the implementation of the PSO Regulation (1370/2007) "*

*Tamás Dombi ZTM Warsaw*

#### About sustainable mobility, energy efficiency and implementation of clean fleets Bucharest 16-18 November 2009.

A series of preliminary questions was raised about the use of low pollution vehicles. Then each partner was invited to share experience about the different aspects of a "green" public transport system, such as:

- > **Green strategies** for energy efficient public transport: assessment of gas emissions, ecological aspects in decision making process, possible overall concepts and specific regulation for green transport, energy efficient driving, ecology and marketing issues, support to the modal shift from car to public transport.
- > **Green technologies** for public transport fleets (bus, trolley bus, metro, regional railway and suburban trains) and future

investment: experience in testing clean vehicles, strategies for alternative energies, fuels and power supply systems.

The workshop enabled partners to get an overview of what occurs in other countries in terms of innovative public transport solutions to fight against pollution and develop sustainable mobility».

#### Conclusion

Thanks to CAPRICE, issues even familiar to EMTA members, are analysed more in-depth, from a technical and practical point of view. Most of all CAPRICE enables a transfer of know-how between the project partners so that identified good practices may be implemented more easily. Extensive exchange across Europe, thanks to European Regional Development funding through the INTERREG IVC programme the partners think, should be encouraged.

More information at:

[www.caprice-project.info](http://www.caprice-project.info)



## Agenda

- **EMTA meeting of the board**  
14-15 January 2010  
Brussels, Belgium
- **EMTA accessibility working group meeting**  
20-21 January 2010 at STIF  
Paris, France
- **EU Transport GHG Routes to 2050**  
Focus Group Meeting  
15 February 2010  
Brussels DG-ENU, Belgium  
[www.eutransportghg2050.eu/](http://www.eutransportghg2050.eu/)  
for registration:  
dominik@ce.nl
- **IT TRANS 2010**  
"Make IT work for public transport"  
Conference & Exhibition  
24-26 February 2010  
Karlsruhe, Germany  
[www.it-trans.de/](http://www.it-trans.de/)
- **AENEAS Workshop**  
"Multimodal mobility marketing in an ageing society »  
4-5 March 2010  
Munich, Germany  
[www.aeneas-project.eu](http://www.aeneas-project.eu)
- **EMTA General meeting**  
15-16 April 2010  
Budapest, Hungary  
[www.emta.com](http://www.emta.com)  
[contact@emta.com](mailto:contact@emta.com)



European Metropolitan Transport Authorities

41, rue de Châteauaudun • F-75009 Paris  
Tél. + 33 1 53 59 21 00 - Fax + 33 1 53 59 21 33  
[www.emta.com](http://www.emta.com) ● [contact@emta.com](mailto:contact@emta.com)